

Development Brief Hamilton Road Quarter, Sudbury Town Centre. Policy SD06 Supplementary Planning Document

March 2010



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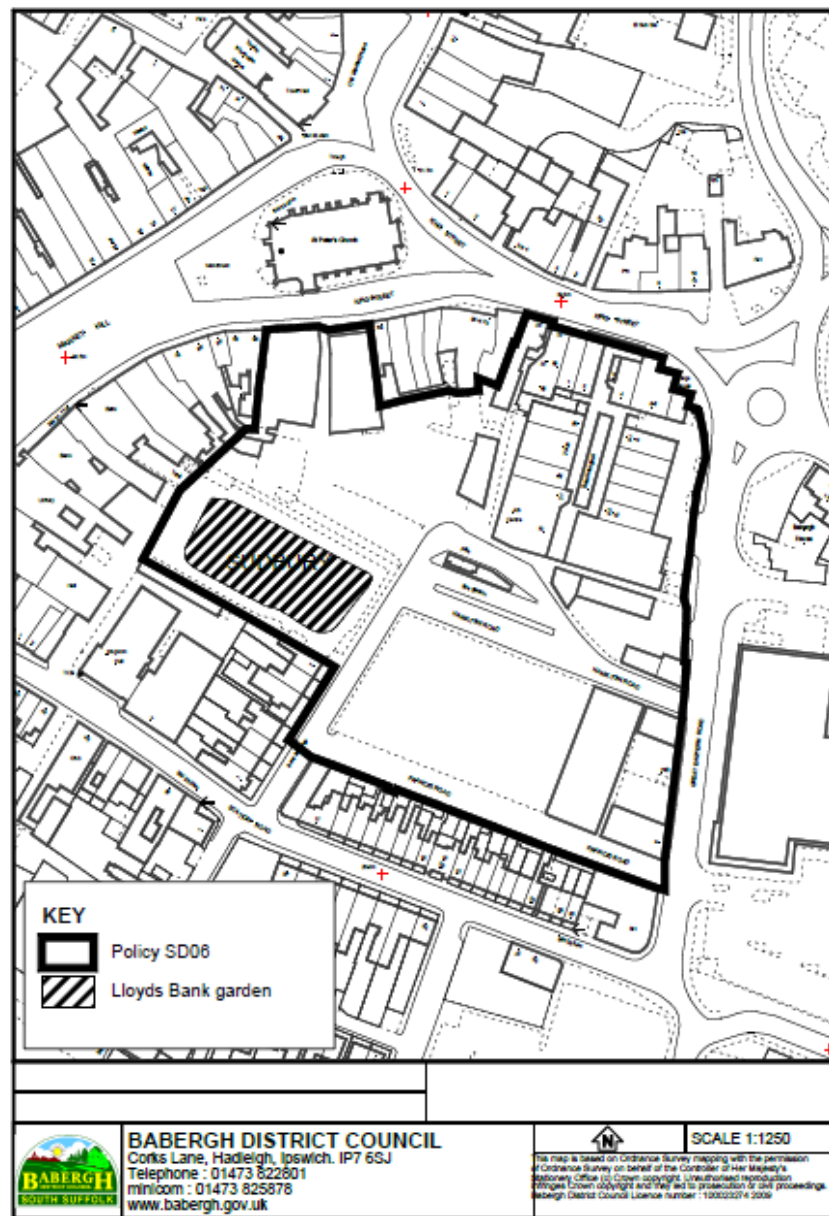
1. INTRODUCTION

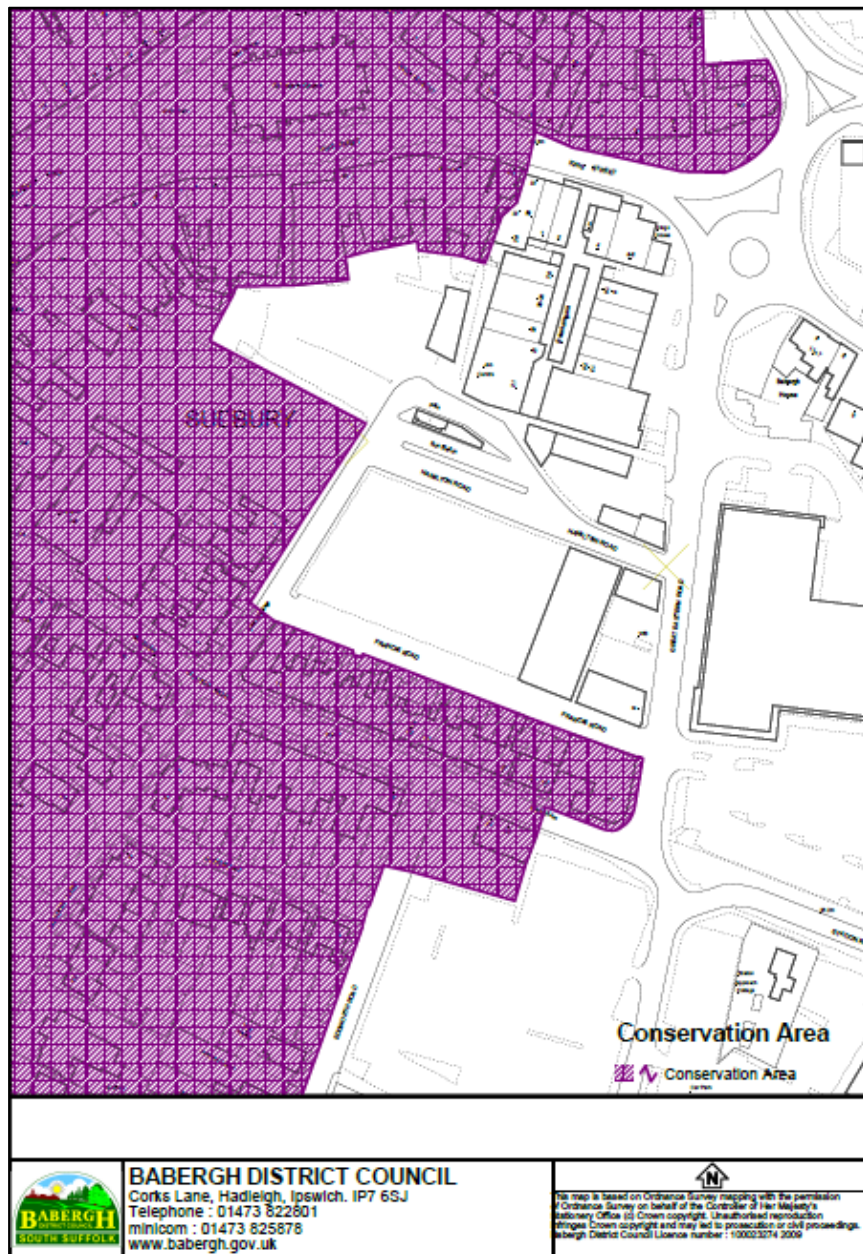
1.1

The Development Brief provides a framework for development within the area of Policy SD06. Its purpose is to provide clear guidance for the appropriate comprehensive redevelopment of the area. It will help developers and applicants draw up ideas for developing the site and provide the Local Planning Authority with a strong context to judge appropriate proposals and importantly, resist development which is not suitable for this important site. It provides a general policy context, a detailed site analysis, a design response to this, in which design principles are set out and finally indicates the parameters for the development process.

1.2 Development Vision

To provide a succinct and overall design statement for the future development of Hamilton Road Quarter. To enable high quality comprehensive redevelopment of this important area of Sudbury Town Centre.





Sudbury Conservation Area

1.3 Background

This Development Brief covers the Policy Area SD06 in the Babergh Local Plan Alteration No 2 (Adopted June 2006). The site area is illustrated on Map 1 above the policy wording states;

1.4 Policy SD06

Land around the Bus Station, Sudbury, is proposed for a mixed- use redevelopment featuring retail, leisure or other commercial uses. Residential development forming an integral part of a mixed- use scheme will be acceptable. The main principles will be:

- **The creation of a safe continuous and attractive pedestrian route between Great Eastern Road and Market Hill, which respects the historic frontage;**
- **When redeveloping the surrounding area to allow for extension of the library;**
- **The retention and improvement of the Bus Station into the scheme, or alternatively relocating to a different, but equally suitable site in the town centre;**

- **Provision of fewer car parking spaces than the maximum permissible under the standards;**
- **The provision of adequate cycle parking facilities;**
- **The implementation of associated highway improvements, as required by the Transport Authority, including the reconstruction of Francis Road, if necessary, to an adoptable standard, with the cost of such work to be borne by the developer; and**
- **Proposals to be of a scale and design that are of high environmental quality and that respect the residential amenity of the nearby dwellings.**

1.4.1 The Babergh Local Plan also makes a commitment to preparing an updated development brief for the area, to help ensure that integrated, viable and sustainable development is secured on this site in the future. In addition Suffolk County Council are committed to improving the Bus Station facility in Sudbury, to ensure that it is able to meet the current needs as well as those predicted for the future having regard to anticipated growth in and around the town of Sudbury. It is important that a comprehensive approach is taken to this, to ensure that opportunities for the site as a whole and opportunities for an improved bus station facility are able to be integrated and complement each other.

1.5 Study Context

1.5.1 History

This area of Sudbury has been the subject of discussion for many years with a number of attempts made to progress redevelopment of the area. Babergh District Council prepared a Development Brief in 1989, which was essentially based on a retail solution for the area, whilst the underlying principles and some elements of this still remain, this requires updating. In 2006 the Civic Trust prepared a Vision Report for the area which included some extensive redevelopment ideas for the area covered by Policy SD06. Also relevant to the development brief, is recent planning history on a significant parcel of vacant land on the site at Francis Road / Hamilton Road. A development proposal for Category II sheltered apartments for the elderly (plus some retail space, landscaping and car parking), was refused planning permission by Babergh District Council in May 2007. It was then defended at Appeal in October 2007, when the Planning Inspectorate dismissed the appeal. Amongst the reasons for dismissal, the following points were included;

- The requirements of SD06 requiring development to comprise retail, leisure and other commercial uses, as well as reference to “limited” residential development, where this forms an integral part of a mixed-use scheme.
- The proposal (subject of the appeal) includes a small element of retail, which is overwhelmingly a housing scheme.
- Concern about the limited contribution the proposal would make to the public realm.
- The development fails to exploit the potential advantage of the bus station location, identified through SD06 as a “key development opportunity”
- Agreement that a development brief for the area is needed to derive an integrated vision for this area that would both attract suitable developers and their customers and create a new urban quarter for Sudbury.
- The development would prejudice the emergence of a coherent development brief able to provide the framework for a high quality

redevelopment of this area, and thus secure the implementation of Policy SD06.

1.6 The Site

- 1.6.1 The SD06 Policy area shown on Map 1 above, includes the Borehamgate Shopping area and also the enclosed garden to the rear of 30 Market Hill, known as Bank Gardens. In addition it includes the wider area between the backs of properties in Market Hill and Great Eastern Road, where Hamilton Road runs through the middle, currently housing the bus station, a few dated retail and commercial operations and a large area of vacant brownfield land.
- 1.6.2 It is this central area which is identified as having the most opportunity to deliver significant economic and environmental enhancement by way of comprehensive redevelopment in the first instance. The focus of this Development Brief is primarily on the Hamilton Road section of the site, although the wider design principles are relevant to the whole Policy area.

1.7 Community context and involvement

- 1.7.1 This Brief has been prepared as a Supplementary Planning Document, under the Planning and Compulsory Purchase Act 2004. As such it has been prepared with significant community involvement and engagement. The steps taken to engage the community on this, went well beyond the minimum requirements of the planning regulations.
- 1.7.2 The policy area includes the existing bus station which is scheduled for improvement by Suffolk County Council. Policy SD06, makes provision for the integration of an improved bus station within the mixed use scheme. For these reasons, Babergh District Council and Suffolk County Council adopted a partnership approach to preparing this Development Brief. During June and July 2009 early consultation and discussions were held with the key stakeholders including;

Sudbury Town Council;
Sudbury Market Town Partnership
Sudbury Chamber of Commerce;
Sudbury Society
Landowners;
Bus Companies and Operators
- 1.7.3 This resulted in 3 options being presented for public consultation as a joint exercise between the District and County Councils, for 6 weeks during the last few months of 2009. Significant publicity, a dedicated web site, 4 public exhibitions and extensive media coverage generated almost 200 responses. These were analysed and considered in January / February 2010. The preferred option has informed the concepts set out in this Development Brief.

2. MARKET DEMAND

- 2.1 In 2008 the Babergh District Retail Study (Colliers CRE), concluded that Sudbury is over-trading in non-food retailing. It concluded that there is a pre-existing need for further comparison goods floorspace and that the requirement for more floorspace will rise steadily through to 2021, on the back of a rising local population and further real increases in spend per head. Any new retail provision should be located within the town centre and should focus primarily on non-bulky comparison goods. A need was also identified for making available larger units which are in most demand from multiple retailers.
- 2.2 Proposals will be required to justify the mix of uses proposed with a market analysis, although it is recognised that market appraisals provide a snap shot at a point in time. Market considerations are obviously an important consideration for delivery, however, the market demand is constantly changing and it is not in itself a justification for one use in place of another.

3. POLICY CONTEXT

- 3.0.1 The overall policy context for this development brief flows from the national, regional and local level. In all cases it is linked to the underlying principles of sustainable development and retention and enhancement of the vitality and viability of the town centre. The main policy drivers are listed below, with a few relevant key objectives highlighted;

- **Planning Policy Statement 1 (Delivering Sustainable Development)**
- **Planning Policy Guidance Note 13 (Transport)**
- **PPS 4 (Planning for Prosperous Economies)**

- 3.0.2 A number of key objectives stem from this national guidance which follows at a regional and local level. Relevant national overarching principles include;

- Sustainable development should be pursued in an integrated manner;
- Approach to development should address potential impacts of climate change, including reducing the need to travel by private car;
- Promote high quality, inclusive design;
- Within town centres, vitality and viability should be promoted,
- Aim to deliver more sustainable patterns of development, ensuring locations are fully exploited through high quality, mixed use development and promoting sustainable transport choices;
- Improve the quality of public realm in the town centres
- New development should help create places that connect with each other sustainably, providing the right conditions to encourage walking, cycling and the use of public transport. People should come before traffic.

3.1 Regional Spatial Strategy

- 3.1.1 The East of England Plan follows the broad sustainable development objectives. In respect of market towns, which include Sudbury, the following policy objectives are relevant;
- Seek to increase the economic and social sustainability of key centres such as market towns;
 - Support urban and rural renaissance;
 - Secure appropriate amounts of new housing, including affordable housing, local employment and other facilities and;
 - Improve the towns accessibility, especially by public transport

3.2 Haven Gateway Sub Region

- 3.2.1 The redevelopment of this area of Sudbury supports the importance of town centre renaissance, which is in line with the Haven Gateway Partnership's Spatial Objectives. The support of the HGP is demonstrated through grant funding under the New Growth Point Fund, to assist with the preparation a Development Brief and as a contribution towards delivery and implementation, for job creation, housing delivery, environmental enhancement, infrastructure and overall vitality and viability of the town centre.

4. SITE AND AREA ANALYSIS



4.1 Historic and visual context

- 4.1.1 Historic maps of this site show a mostly industrial built form occupying this site. The proximity of the railway gave rise to a collection of warehousing and maltings fed by railway sidings. Evidence of this occupation is now negligible.

- 4.1.2 Whilst within the heart of the historic core of Sudbury and bounded, on three sides, by the town centre conservation area this site has little remaining heritage. To the south is Francis Road and the back of the terrace of brick built weaver's cottages facing Station Road. The front elevation of these cottages adds to the historic quality of the town centre but the backs are dominated by private parking and a collection of personalised boundary treatments that have little aesthetic value but are typical backland character. The road itself is privately owned and provides necessary access to the parking for these houses. Access for development on adjacent land should be negotiated for rear servicing as frontage development here would not be appropriate.
- 4.1.3 The two storey terraces known as Bank buildings to the west of the site provide the strongest context and warrants sympathetic response within the development site. Adjacent to Bank Buildings is the red brick wall of a historic garden for a house facing Market Hill. Now a bank, the house and its high walled garden are important historically. Several protected trees stand inside the wall and add greatly to the privacy and enclosed character of the garden. English Heritage has stated the wall and garden should remain as a private space but could be opened to the public providing no loss of character or historic fabric occurred.



Bank Buildings

- 4.1.4 To the north is the service yard of shops on Market Hill and beyond this the backs of the shops. This area has a purpose and function which reflects its appearance and necessity. Opportunities to aesthetically enhance this area are limited by access demands and the need for this area to function properly for the premises it was created for.
- 4.1.5 The mid twentieth century shopping precinct Borehamgate forms the boundary to the north eastern corner of the site. Unfortunately this development with its internalised access to small retail units turns its back on the town and has a negative effect on the streetscape and character of this part of the town centre. The internal 'mall' has an entrance from the SD06 site but this provides little legibility.



Borehamgate Precinct

- 4.1.6 This negative effect extends down Great Eastern Road which is a visually poor street. There are few frontages along its length apart from the low quality buildings within the SD06 boundary. A large supermarket also turns its frontage away from this street on the eastern side. The poor aesthetic value of this street is not improved by the significance of the road network and the large amount of traffic that uses it. This is without doubt the poorest street of the town centre with regard to townscape and historic context. Although the SD06 site has the opportunity to create frontages along this street the weakness of the whole street would continue to negate regenerative intentions.
- 4.1.7 The site context is considered to be ambiguous and weak apart from the western boundary where the listed Bank Buildings merit a sympathetic design response. Within the SD06 site the generally poor quality of buildings and uses, are the obvious justification for this development brief. The 20th century interventions to the north and east have created large flat roofed buildings of a scale and mass more associated with the site's industrial past than the more domestic and fine grained scale of Market Hill and Station Road.

4.2 Movement and connections

4.2.1 Pedestrians

There are significant movement patterns that relate to the existing occupation of the site as well as the routes through the site from the large scale parking facilities to the south and the attractors within the shopping core. One of the visually least satisfactory but strongest desire lines for pedestrians draws movement through Hamilton Road and the bus station, then into Market Hill via the small pedestrian lane between the Winch and Blatch shops in the North West corner of the site. This route appears like a back door to the town, despite its popularity.



Route through Winch and Blatch

4.2.2 The southern entrance to Borehamgate is a second movement corridor between the core and the SD06 site. Both these two routes offer a more direct and visually better and less car dominated route than following Great Eastern Road north to the centre.

4.2.3 Public transport

The bus station also generates pedestrian movement into the site from all routes. Whilst accessible for the town centre the bus station feels tucked away at the rear of the site. As an important facility this location creates a poor sense of place and arrival for public transport. The layout arrangement of the bus station within Hamilton Road makes for a complicated pattern of vehicle movements. In this arrangement Hamilton Road is a one way street carrying buses and service vehicles back on to the main road network that have accessed the SD06 area from the south of Bank Buildings. The vehicle movements and multiple lanes within the bus station area dominate pedestrian desire lines.

4.2.4 Vehicular access

Service vehicles and employees from Market Hill access the service areas on the one way route north, past Bank Buildings and the exit east through Hamilton Road. Private cars also use the same, one way circuit as described for the bus movements. The circular route around the whole site is unfortunate in that it is the 'long way round' so to speak and involves all vehicles passing through the bus station area, adding to the dominance of vehicles at the expense of the pedestrian and bus user experience.

4.2.5 Parking

There is currently no parking within the site other than the staff parking at the back of Market Hill and Francis Road which provides private parking for the residents to the south. Both these parking areas could be rationalised but are, and will remain, the rear of property with the associated functions and constraints.

4.2.6 Francis Road is a private road that provides parking for the houses that front Station Road. There is no restriction on public access through this street.



Francis Road

4.3 Urban Design appraisal

4.3.1 Landscape and topography

The site has a gradual fall to the south east and has no overall issues concerning levels. As a brownfield site there is little landscape of adequate value to be retained. The trees within the walled garden of Lloyd's Bank are an important landscape feature that can be seen from direct views along Hamilton Road. The pedestrian access through Winch and Blatch land rises away from the bus station to Market Hill. Where this rise meets the wall on Lloyd's Bank garden there is a vertical change of perhaps a metre, supported by the garden wall. This gradual rise to the town centre and the significant landmark of the Church Tower in Market Hill are important in reading the way to the heart of the town.

4.3.2 Public and private space

Francis Road and the service yard to the north of the site are at the back of property and have functions that take place in private space. There is some enclosure to the service yard at the north of the site to contain its function but the activities of parking and waste storage in Francis Road happen in an ambiguous space not clearly private or public. Whilst it is in private ownership the open public access make this an unattractive street.

4.3.3 The bus station can also be seen to have a spatial definition problem, partly because of the layout and also because the other traffic that passes through its indistinct area of operation.

4.3.4 Continuity and enclosure

The policy for SD06 requires a comprehensive redevelopment of this site. The redevelopment would therefore seek to achieve appropriate enclosure in response to the context of each boundary.

4.3.5 Legibility

St Peters Church tower in Market Hill provides the strongest landmark seen from the development site and the south beyond. The current development within the site provides few clues in interpreting the finer grain of desire lines that exist or how to navigate through the site. The bus station feels tucked away and may be difficult to find for the first time user. Great Eastern Road

has such limited frontages that it also provides little help in understanding the proximity of the centre of the town.



Poor legibility through bus station

4.3.6 Scale and massing

Development around the site is a mixture of two and three storey buildings. Bank Buildings are two storey and the terrace of Francis Road is two and three storeys. The mass of these two forms is residential in scale unlike the later buildings of Roy's supermarket and Borehamgate which are larger box like structures of an unrelated massing. Their scale and height are similar to the other forms around the site.

- 4.3.7 The development in Market Hill has a domestic scale of two and three storeys, the servicing yard for these buildings provides some separation from the developable area of SD06 but even with the slightly elevated position the massing is similar to other boundaries.

4.3.8 Character and distinctiveness

The most notable distinction of character areas is the fine grain and detail of the historic buildings against the stark vulgarity of the later interventions of the 20th century. The only buildings of historic character that face the site are Bank Buildings. Francis Road is of the same grain but, as previously considered, the terraces of housing backs onto the development site and the quality and function of the back of the terrace is of little visual or historic quality.

- 4.3.9 Great Eastern Road and Borehamgate provide a poor character and context to the town and this site.

4.4 Existing Land Uses and activity

4.4.1 Activity

Currently the site is occupied by the bus station in the North West corner. The station generates a lot of activity, especially in peak times. Many people also pass through the site using the route to the centre via the lane between Winch and Blatch shops. From Great Eastern Road the road crossing

generates significant movement into the site, although without the bus station, people are mostly passing through the site, into the centre or returning out.

4.4.2 The Bus Station

The bus station is an important part of Sudbury's infrastructure, providing a sustainable means getting to the town for many people. The layout and facilities are acknowledged as tired and funding exists to provide improvements to bring the station up to current standards. The location has the advantage of being very close to the town centre, making much of the town accessible within the 400m walking distance generally accepted as the optimum walking distance from facilities. The location is, however, a poor one. This is because it is difficult to find, in an area that does not read as a principle route or street. The movements of both busses and other vehicles through the station dominate pedestrian flows and access making the operation not particularly user friendly.

4.4.3 The relationship of the bus station to the railway station has important sustainable aspirations to act as an interchange for connecting further journeys. The present location of the bus station disadvantages the interchange potential and this is again because the bus station is hidden, from legible routes or connections.

4.4.4 The bus station layout and capacity proposed is considered to adequately serve existing and predicted future use taking into account likely growth in and around the town.

4.4.5 Residential

There is no residential use of the site

4.4.6 Commercial / retail

The retail uses of the site consist of a single retail outlet. There are two car repair/sales operations on the Great Eastern Road frontage. There is also a small service use operating as a dry cleaners and an office.

4.4.7 Land Ownership and Tenure

The redevelopment site is currently in 4 ownerships, one of which is Babergh District Council. The opportunities for development of the site reflect the existing ownerships, although it is anticipated that negotiations between landowners and developers will lead to land assembly as appropriate, enabling delivery of comprehensive redevelopment.

4.4.8 Public adopted highway

With the exception of Francis Road all the streets and the Bus Station are adopted highway.

4.4.9 Buildings and new development

There has been no new development within the site. The buildings on the site are considered to be of poor architectural value and with regard to the town centre context comprehensive development would offer much more than including the existing built form in redevelopment.

4.4.9 Infrastructure and services

The infrastructure for statutory services is laid within the highway. There are no significant easements known at this time.

5. DESIGN RESPONSE

The Hamilton Road Quarter represents the most significant opportunity to improve and expand the heart of Sudbury. The current uses, with the exception of the bus station, are not in a good layout or in some cases unsuitable for a town centre location. Uses in single storey buildings should be considered spatially inefficient or undesirable within the town centre.

The vacant land and lack of well designed streets make the quarter highly suited to comprehensive redevelopment through which a much stronger sense of place can be established. The bus station has an important role in bringing people to the town and should remain within close proximity to the centre so that the people using buses are not discriminated against in the redevelopment of the site. The access for both buses and pedestrians needs to be reconsidered to optimise the function and presence of the bus station as an integral part of the redevelopment and as an interchange with the railway station.

Final Plan



There is a significant desire line for pedestrians through the site from the southern parking area around the Kingfisher Leisure Centre, Waitrose, Roy's and the large car parks to the south. This desire line provides a substantial amount of footfall for new retail opportunities in Hamilton Road. The redevelopment should make this desire line a new street that provides retail experience appropriate to the centre of town. Other uses should not be discounted and the opportunity to provide a leisure use such as a cinema above ground floor uses may provide an evening economy with associated cafes and restaurants within the quarter.

Rationalising the location of the bus station within the site and the way that vehicles gain access to the service areas at the back of Market Hill will create a pedestrian realm for the length of Hamilton Road linking to Market Hill via the Winch and Blatch lane between shops. In this new space within Hamilton Road a high quality public realm can be established adding value and vitality to the whole town.

The new quarter can be far more than a route to the centre. The development of a new pedestrian area, fronted by new shops and restaurants gives the town centre a new identity and potentially a place in which an evening economy can prosper. Within an exciting mixed use environment the people of Sudbury can rendezvous, shop and dine in a well designed, sunny public space or within the new buildings enclosing the street.



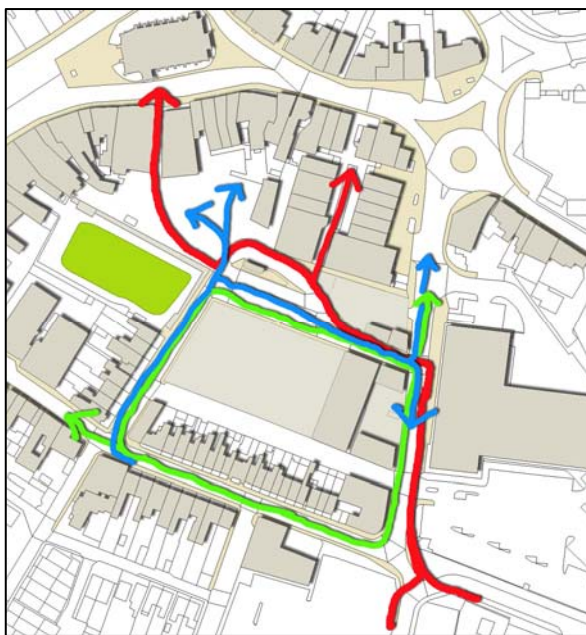
Pedestrian area/ public realm (illustrative)

5.1 Design principles

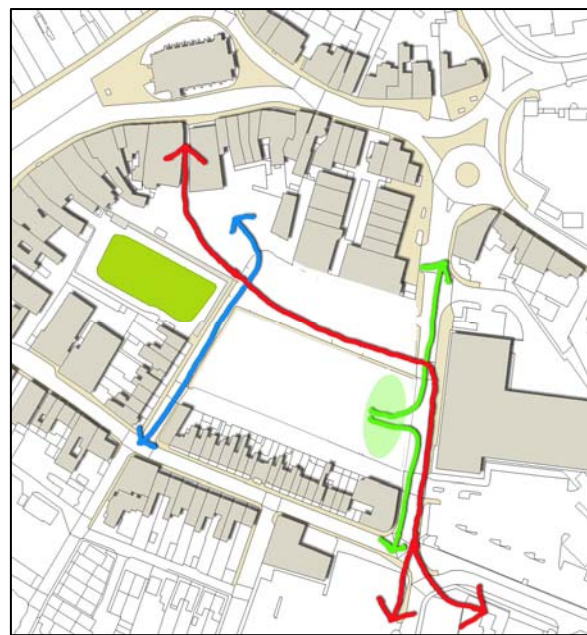
The redevelopment of the Hamilton Road Quarter will need to achieve the following principles based on the analysis of the site, its context and opportunities the location makes available. These principles also respond to the extensive public consultation that took place in November 2009 and take into account the most significant concerns and desires raised by the residents of Sudbury. The requirements of Local Policy SD06 are also encapsulated in the principles. The policy is set out in full in paragraph 1.4 above. The policy principles are addressed below.

5.2 Provide a more functional and legible bus station as part of the scheme, or alternatively locating to a different, but equally suitable alternative site in the town centre.

5.2.1 The bus station provides sustainable and, for some, a vital means of transport. In association with Suffolk County Council a new location for the bus station has been agreed within the site. This new position does not represent a loss of proximity to the town centre or prejudice bus users by increasing the distance from the centre. The new location does allow for buses to enter and leave the site directly from Great Eastern Road. This entrance may require a bus only right turn lane in Great Eastern Road. This would require a traffic light control system that is linked to the pedestrian crossings. The new entrance removes bus traffic from Hamilton Road and simplifies the movement of buses taking pressure of the residential street at Bank Buildings. This decrease in vehicle movements allows for the instatement of a two way carriageway so that other traffic can also be removed from Hamilton Road. A traffic light control system is required on the Station Road junction to enable this. Hamilton Road will then become a pedestrian area, only requiring access for emergency vehicles.



Current Movement Patterns

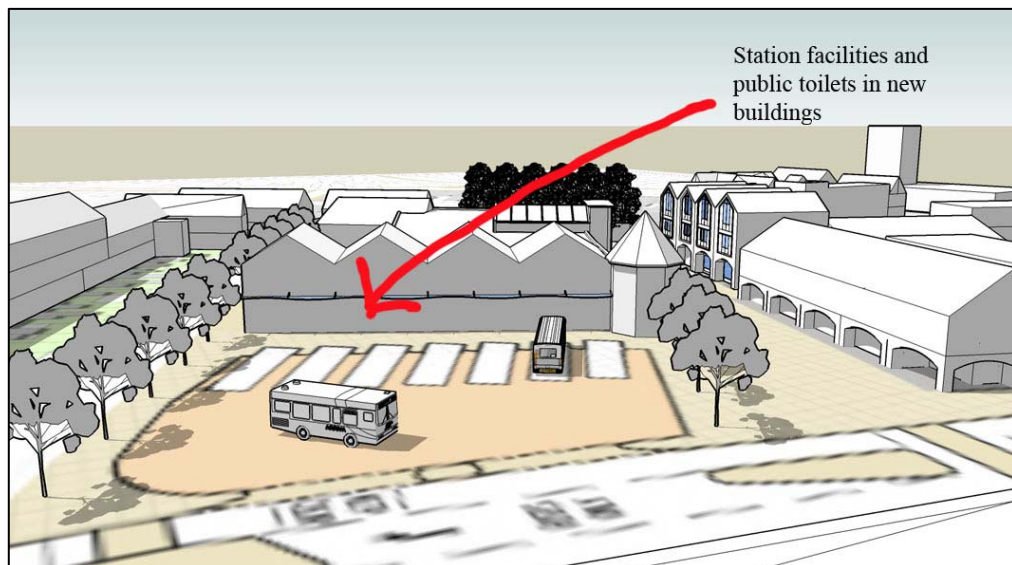


New Plan Movement Patterns

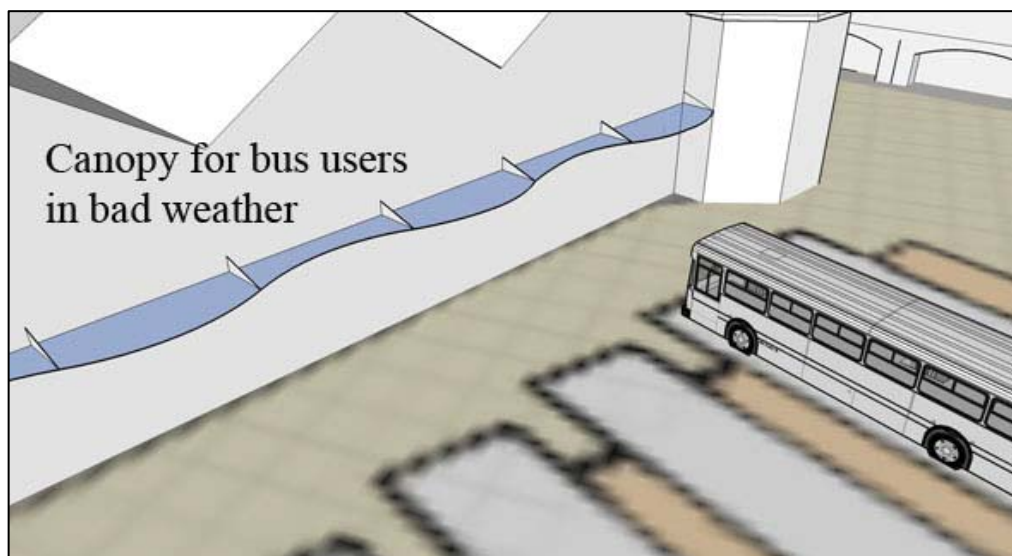
5.2.2 The new location of the bus station provides a stronger and more positive sense of arrival in the town and a more obvious and direct connection to the railway station. The provision of real-time train information at the bus station will further strengthen the interchange facility.

5.3 Facilities for bus users and staff must be integrated into the new built form.

- 5.3.1 The bus station will have new facilities integrated into the new development directly facing the station. This should include a waiting room, public toilets, ticket office and tourist information. Facilities for bus operators may also be required. The new development shall provide canopy shelters for bus users along the frontage where the bus bays are accessed by the public.



Station View



Canopy Illustrations

- 5.3.2 Suffolk County Council will ensure the bus turning area has separation from the pedestrian area with a barrier design which integrates into the overall furniture design for the public realm.

5.4 The creation of a safe, continuous and attractive pedestrian route between Great Eastern Road and Market Hill.

- 5.4.1 The removal of traffic from Hamilton Road creates the opportunity for an entirely pedestrian street between new buildings. This street should be wide enough to hold events and activities for the public and also provides the opportunity for street cafes, especially on the north side where the sun will provide a suitable environment. The continuation of the pedestrian realm will continue through the land owned by Winch and Blatch leading to Market Hill.

It is important that detailed design makes this route legible and safe with a strong sense of pedestrian priority for its entirety. The infrastructure for CCTV and events in the public realm will be an integral part of the development.

5.5 Create an appropriate mix of uses

- 5.5.1 All buildings should be designed in a robust way so that all storeys are capable of adaptation for different uses. Uses that generate vitality and activity of the ground floor of all buildings will be encouraged. A1 and A3 should dominate the ground floor of developments in Hamilton Road to create and exploit the vitality of the public realm during the day and into the evening. Office and other uses that do not contribute to street vitality uses will be restricted to upper storeys.
- 5.5.2 Buildings should be designed so that restaurants and cafes can occupy the ground floor where appropriate and architectural design should make suitable provision for fume and odour extraction that does not detract from the quality of the buildings or inconvenience occupiers of upper storeys.
- 5.5.3 Inclusion of residential development will be required opposite Bank Buildings to recreate a traditional residential street. An element of residential development can be considered on upper storeys to Hamilton Road subject to a satisfactory market analysis of demand. Residential development of more than 15 Units (0.5 hectares) will require 35% to be an affordable housing provision. On sites providing 3 or more dwellings this affordable housing provision will be negotiated.
- 5.5.4 A leisure use that provides family entertainment and evening activity such as a cinema will be encouraged to occupy an upper storey of the southern part of the site. This is also linked to a wider priority of Babergh's Community Safety Partnership, of creating a safer night-time economy in Sudbury. An evening use would benefit from the local car parks having little use in the evenings and therefore would not require additional car parking provision.

5.6 Greening the new development

- 5.6.1 As there is no frontage development along the length of Great Eastern Road the development of frontage on this site alone cannot repair the street. The new location for the bus station has a high degree of function but does not provide a frontage. Instead of using built form to provide enclosure and a landmark feature in Great Eastern Road trees will be used as a substitute around the bus station. Any new building for example a cinema or shop outlets would also do this, but be set back from the Great Eastern Road frontage. As the trees mature they can provide a strong landmark and visual aid to recognising the location from a distance. These trees have a key role in the quality of this development and their implementation should take primacy over underground infrastructure.



Value of trees

5.7 Improvements to Francis Road

- 5.7.1 Francis Road will provide access for servicing and very limited car parking for the new developments on the southern side of Hamilton Road. This additional use of this street may require adoption by the Highway Authority and improvements to the layout and current parking arrangements. Trees should be planted on the northern side of Francis Road for its length to improve the aesthetic, mitigate the private parking and backland activity as well as provide character and enclosure between the backs of existing houses and the back of the new development. A means of enclosure at each end of Francis Road will be required to suggest that this is not a public through route. The cost improvements to Francis Road will be borne by the developer, provided this is reasonable and necessary.

5.8 Proposals to be of a scale and design that are of high environmental quality and that respect the residential amenity of the nearby dwellings.

- 5.8.1 The site analysis has revealed that the immediate context for this site is generally poor. With the exception of Bank Buildings there are no significant buildings that suggest certain architectural response for the whole development. The historical pattern of previous development was that of industrial scale and mass and this may have justified the scale and massing of Roy's supermarket and Borehamgate. The new development should aim to protect views to St Peter's Church from the south. Generally development should not exceed three storeys, although it may be appropriate to do so in part, if there is an appropriate opportunity to create a landmark feature for identity or a sense of place. The scale and mass of the new development must be sympathetic to the historic form of Sudbury's town centre. The context does not suggest a particular architectural style but a strong identity and character is essential for the whole site as the scheme will be a new quarter of the town centre. The design approach must be explained within a Design and Access Statement.



Bank buildings and bank building with terrace

- 5.8.2 The landscaping of the public realm shall be to the highest standards. Material for the hard landscape shall define the strong desire line through Hamilton Road and the Winch and Blatch land to Market Hill. The new pedestrian realm shall be designed to allow street cafes to function without compromising movements through the street. This street will have seating for public use as well as the opportunities for cafes and restaurants.



Attention to detail

- 5.8.3 The areas of seating should include a high degree of decorative materials, planting and public art to bring richness in places where people have time to appreciate these qualities.

5.9 Art in the town centre public realm

- 5.9.1 Public art is art that gives a location a sense of identity, engages people in ideas or feelings about a place, and is located in areas that are accessible to the public. Public art may be permanent or temporary, free standing or an integral part of the design, it may make a bold statement or be an intimate detail.
- 5.9.2 An artist should ideally be engaged by the developer at the earliest opportunity to develop a strategy and series of public art works to bring a cultural richness to the development.

5.10 Provision of fewer car parking spaces than the maximum permissible under the standards and the provision of adequate cycle parking facilities.

- 5.10.1 For sustainability reasons the provision of car parking on this site shall be below the standard provision. The central location and facilities immediately available give rise to this relaxation.
- 5.10.2 Cycle parking for the bus station and the town centre shall be incorporated into the public realm where it is not intrusive but has natural surveillance and security.

5.11 Sustainable Design and Construction

- 5.11.1 The comprehensive new development will have a high degree of sustainable attributes. Babergh District Council will encourage high levels of sustainable construction techniques and standards for the life of the buildings. Policies EN09 and EN10 in the Babergh Local Plan will be relevant as will the objectives set out in Policy ENG1 on Carbon Dioxide Emissions and Energy Performance in the East Of England Plan.

6. DEVELOPMENT PROCESS

- 6.1 As a Development Brief it is helpful to provide guidance about the development process with an indication of the likely requirements issues concerning delivery. At this stage it is not appropriate to prescribe the detail of matters linked to delivery requirements, however the list below indicates the considerations which will be the subject of negotiation in relation to bringing forward comprehensive development on this site. This is not exhaustive, but provides an indication of the key issues which are likely to apply;

6.2 Phasing

Agreement with Babergh District Council about the need for phasing development will be required as early in the development process as possible. Early phases of redevelopment will need to be planned with regard to their potential impact on subsequent phases, as it is anticipated that further parcels of land are likely to come forward as later elements of the overall scheme.

6.3. Planning Application Requirements

It is likely that a range of supportive documents will be necessary to support any planning application(s) for redevelopment of the site, possibly including, but not necessarily limited to, the following;

- Design and Access Statement
- Transport Assessment / Green Travel Plan
- Supporting Planning Statement
- Financial Viability information
- Section 106 proposals
- Ecological Assessment
- Archaeological and Heritage Impact Statement

Detailed requirements for supportive planning application material will be determined by the precise nature and scale of application proposals.

6.4 Infrastructure

It will be necessary to agree the requirements for infrastructure provision (some of which may be necessary in advance) associated with proposed development. Discussions in relation to this should commence as early as possible with Babergh District Council, together with infrastructure providers as appropriate.

6.5 Planning Obligations

When development comes forward on this site, there are likely to be a number of requirements which will form the subject of a planning obligation. The detail of this will depend on the detailed nature and proportions of the uses proposed. Based on the relevant tests, the potential nature of contributions or measures which may be appropriate are outlined below, some examples of these include;

- Public realm contributions including landscaping / tree planting other environmental enhancement
- Affordable housing (where is residential included)
- Infrastructure improvements
- Highways and access arrangements
- Facilities and infrastructure for sustainable travel options, eg cycle parking.
- Safety measures such as CCTV

6.6 Other Requirements and Considerations

Other requirements that may be relevant include;

Biodiversity appraisal of cleared site

Environmental Impact Assessment (Subject to screening and size and nature of the scheme).

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